VERMONT

State of Vermont
Contract Administration
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Agency of Transportation
Finance & Administration

[phone] 802-622-1285
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December 5, 2024

RFP ADDENDUM # 3

RE: Request for Proposals – Aviation Fuel Storage and Dispensing System and Installation Services Middlebury Airport, Middlebury, Vermont

The Request for Proposals (RFP) for the above-mentioned services has been modified to replace the Plans on pages 16-20 of the RFP with the Plans attached hereto and made part hereof.

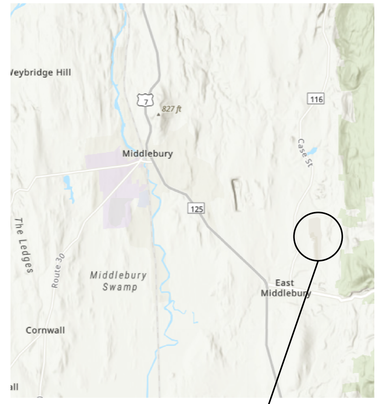
Sincerely,

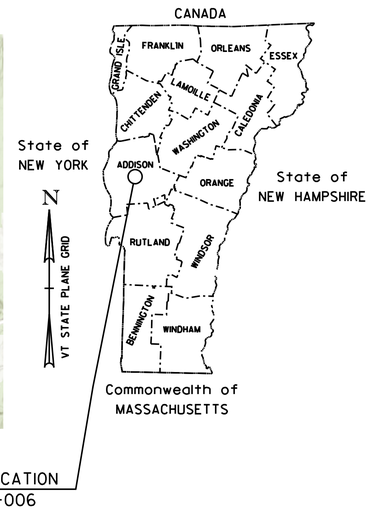
Caryn Pletzer

Caryn Pletzer
Contract Administration

cc: Contract Manager
Project File

STATE OF VERMONT
AGENCY OF TRANSPORTATION





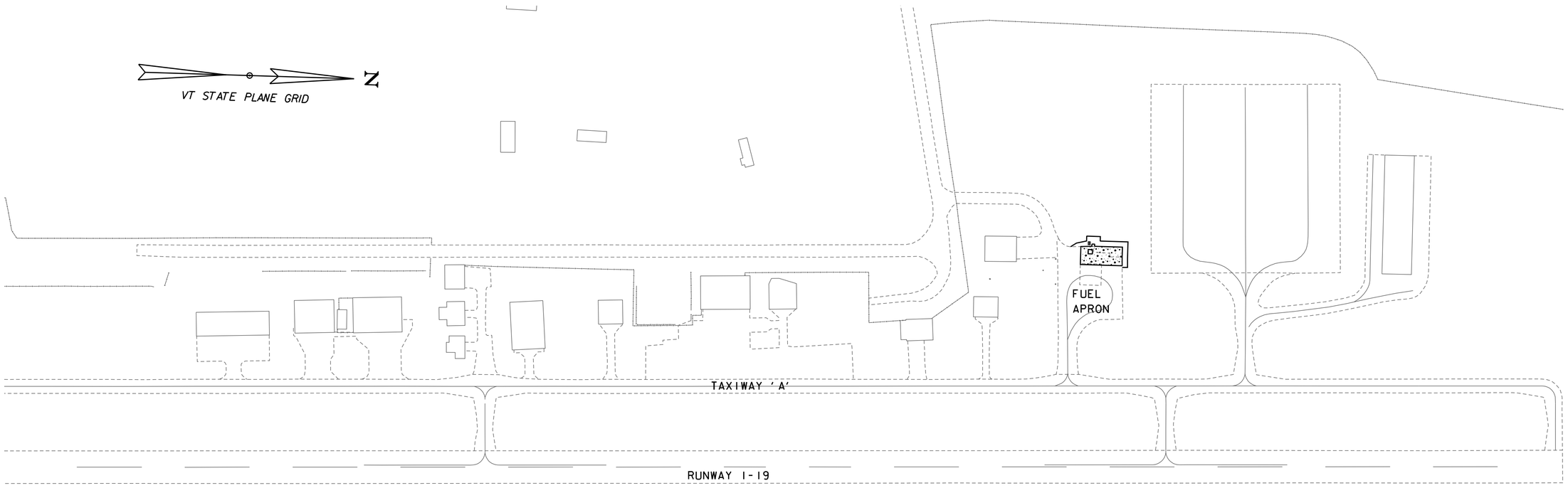
PROPOSED IMPROVEMENT

TOWN OF MIDDLEBURY

COUNTY OF ADDISON

MIDDLEBURY STATE AIRPORT

PROJECT DESCRIPTION : UNDERGROUND STORAGE TANK REPLACEMENT AND SPCC PLAN



SCALE 1" = 100'-0"
100 0 100

QUALITY ASSURANCE PROGRAM : LEVEL X

SURVEYED BY : N/A

SURVEYED DATE : N/A

DATUM

VERTICAL NAVD 88

HORIZONTAL NAD 83 (1996)

THE WORK SHOWN ON THESE PLANS IS CONCEPTUAL IN NATURE AND IS INTENDED TO BE USED FOR THE DEVELOPMENT OF PROPOSALS. FINAL DESIGN SHALL BE COMPLETED BY THE CONTRACTOR. THE CONTRACTOR SHALL PROVIDE DESIGN BUILD DOCUMENTS STAMPED BY A PROFESSIONAL ENGINEER OR ENGINEERS, REGISTERED IN THE STATE OF VERMONT. THE CONTRACTOR PROFESSIONAL ENGINEER OR ENGINEERS SHALL BE CONSIDERED THE ENGINEER OF RECORD FOR ALL WORKS CONSTRUCTED WITH THE FUEL FARM.

Jacobs

STATE OF VERMONT
JAMES ANTHONY MURPHY
No. 6199
PROFESSIONAL ENGINEER
8/30/2024

STATE OF VERMONT
MATTHEW Z. RAUH
NO. 134940
ELECTRICAL
LICENSED
PROFESSIONAL ENGINEER

NOT FOR CONSTRUCTION

VERMONT AGENCY OF TRANSPORTATION
RAIL & AVIATION BUREAU DIRECTOR

APPROVED Dan Delabruere DATE Sep 05, 2024

PROJECT MANAGER : SASA DEJAN

PROJECT NAME : MIDDLEBURY

PROJECT NUMBER : AV-FY23-006

SHEET 1 OF 5 SHEETS

N

VT STATE PLANE GRID

SAW CUT LIMITS OF PAVEMENT REMOVAL (TYP.)

EXISTING EDGE OF PAVEMENT (TYP.)

EXISTING FUEL FARM PREVIOUSLY REMOVED

REMOVE CONCRETE PAVEMENT (TYP.)

REMOVE PAVEMENT (TYP.)

EXISTING QTPOD SYSTEM WILL BE REUSED/SALVAGED

REMOVE BOLLARD (TYP.)

EXISTING CATCH BASIN DO NOT DISTURB

EXISTING TAXIWAY CENTERLINE (TYP.)

EXISTING DRAINAGE PIPE DO NOT DISTURB (TYP.)

EXISTING FUELING APRON

EXISTING CONCRETE PAD

50'

REMOVE EXISTING FUEL FARM POWER CABLES FROM UNDERGROUND CONDUIT BACK TO EXISTING EMERGENCY SHUT OFF SWITCH. ABANDON CONDUIT IN PLACE.

BARRICADES PROVIDED BY VTRANS (TYP.)

ESO

REMOVE EXISTING JUNCTION BOX FROM BASE OF EXISTING UTILITY POLE. EXISTING WIRING AND CONDUIT TO FLOODLIGHT ON UTILITY POLE TO REMAIN.

EXISTING MANHOLE TO REMAIN

EXISTING CONTOUR (TYP.)

REMOVE EMERGENCY SHUT OFF SWITCH, SUPPORT, AND ABOVE GROUND CONDUITS, CUT AND CAP CONDUITS BELOW GRADE.

REMOVE EXISTING FUEL FARM AND FLOODLIGHT POWER CABLES FROM UNDERGROUND CONDUIT, EXISTING CONDUIT TO REMAIN.

EXISTING FLARED END SECTION

EXISTING METER SOCKET LOAD CENTER ADJACENT TO EXISTING ELECTRICAL UTILITY POLE TO REMAIN.

HANGAR

HANGAR

ACCESS ROAD TO AIRPORT ROAD

FENCE (TYP.)

CONTRACTOR ACCESS

TAXIWAY 'A'

RUNWAY 1-19

SCALE 1" = 20'-0"

20 0 20

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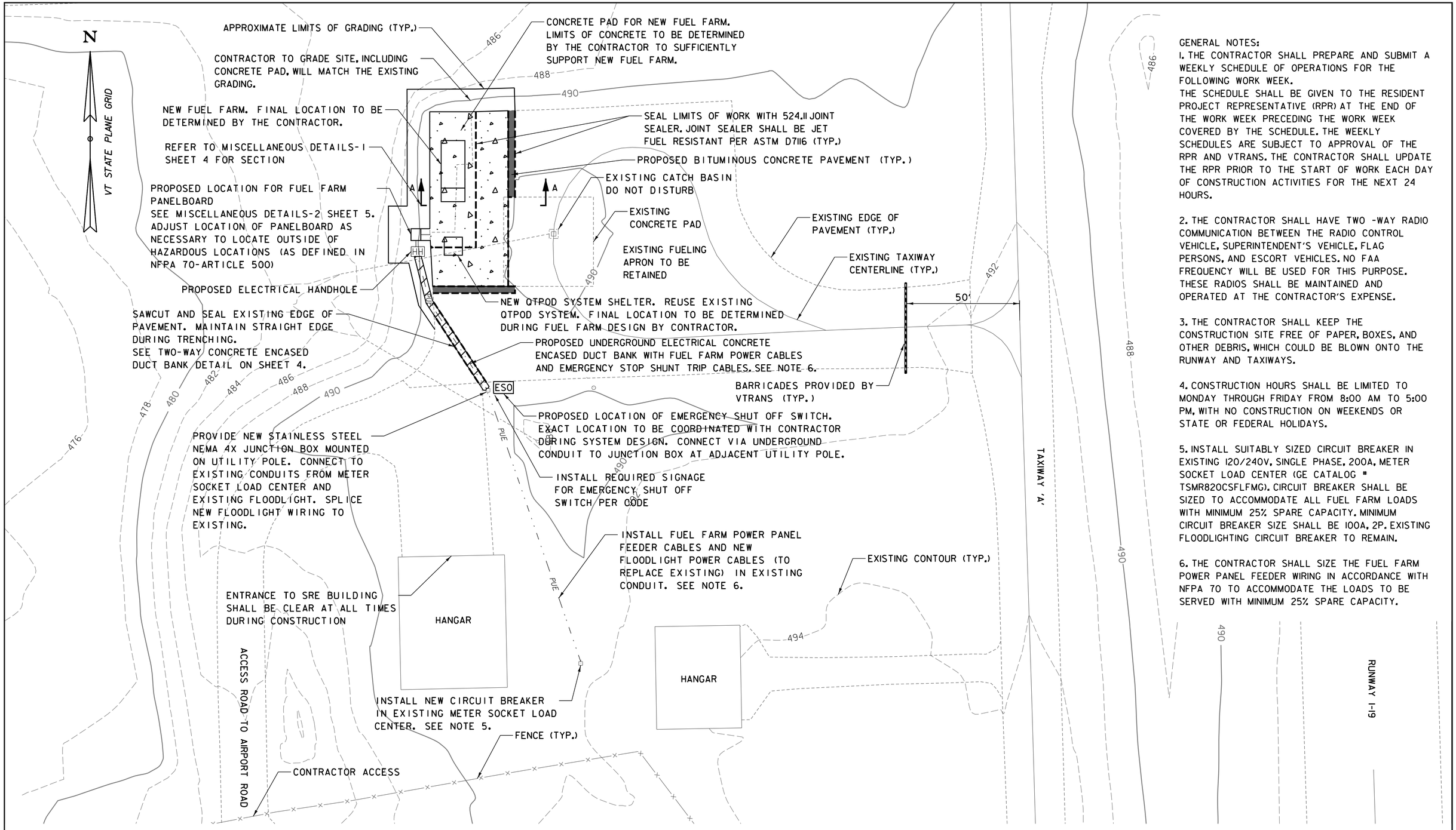
Jacobs

PROJECT NAME:	MIDDLEBURY
PROJECT NUMBER:	AV-FY23-006
FILE NAME: z17H107bdr_ex.dgn	PLOT DATE: 8/30/2024
PROJECT LEADER: J. HEHIR	DRAWN BY: S. BAUMANN
DESIGNED BY: S. BAUMANN	CHECKED BY: J. HEHIR
SITE PREP PLAN	SHEET 2 OF 5



SCALE 1" = 20'-0"
20 0 20

GENERAL NOTES:

1. THE CONTRACTOR SHALL PREPARE AND SUBMIT A WEEKLY SCHEDULE OF OPERATIONS FOR THE FOLLOWING WORK WEEK.
THE SCHEDULE SHALL BE GIVEN TO THE RESIDENT PROJECT REPRESENTATIVE (RPR) AT THE END OF THE WORK WEEK PRECEDING THE WORK WEEK COVERED BY THE SCHEDULE. THE WEEKLY SCHEDULES ARE SUBJECT TO APPROVAL OF THE RPR AND VTRANS. THE CONTRACTOR SHALL UPDATE THE RPR PRIOR TO THE START OF WORK EACH DAY OF CONSTRUCTION ACTIVITIES FOR THE NEXT 24 HOURS.

2. THE CONTRACTOR SHALL HAVE TWO -WAY RADIO COMMUNICATION BETWEEN THE RADIO CONTROL VEHICLE, SUPERINTENDENT'S VEHICLE, FLAG PERSONS, AND ESCORT VEHICLES. NO FAA FREQUENCY WILL BE USED FOR THIS PURPOSE. THESE RADIOS SHALL BE MAINTAINED AND OPERATED AT THE CONTRACTOR'S EXPENSE.

3. THE CONTRACTOR SHALL KEEP THE CONSTRUCTION SITE FREE OF PAPER, BOXES, AND OTHER DEBRIS, WHICH COULD BE BLOWN ONTO THE RUNWAY AND TAXIWAYS.

4. CONSTRUCTION HOURS SHALL BE LIMITED TO MONDAY THROUGH FRIDAY FROM 8:00 AM TO 5:00 PM, WITH NO CONSTRUCTION ON WEEKENDS OR STATE OR FEDERAL HOLIDAYS.

5. INSTALL SUITABLY SIZED CIRCUIT BREAKER IN EXISTING 120/240V, SINGLE PHASE, 200A, METER SOCKET LOAD CENTER (GE CATALOG # TSMR820CSFLFMG). CIRCUIT BREAKER SHALL BE SIZED TO ACCOMMODATE ALL FUEL FARM LOADS WITH MINIMUM 25% SPARE CAPACITY. MINIMUM CIRCUIT BREAKER SIZE SHALL BE 100A, 2P. EXISTING FLOODLIGHTING CIRCUIT BREAKER TO REMAIN.

6. THE CONTRACTOR SHALL SIZE THE FUEL FARM POWER PANEL FEEDER WIRING IN ACCORDANCE WITH NFPA 70 TO ACCOMMODATE THE LOADS TO BE SERVED WITH MINIMUM 25% SPARE CAPACITY.

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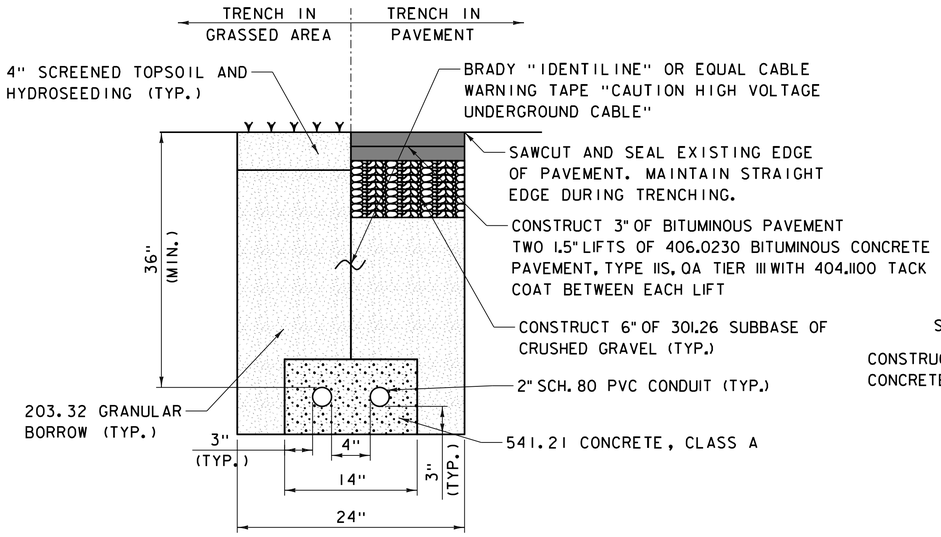
NOT FOR CONSTRUCTION

Jacobs

PROJECT NAME:	MIDDLEBURY
PROJECT NUMBER:	AV-FY23-006

FILE NAME: z17h107bdr.dgn		PLOT DATE:	8/30/2024
PROJECT LEADER: J. HEHIR		DRAWN BY:	S. BAUMANN
DESIGNED BY:	S. BAUMANN	CHECKED BY:	J. HEHIR
SITE PLAN		SHEET	3 OF 5



DUCT BANK NOTES:

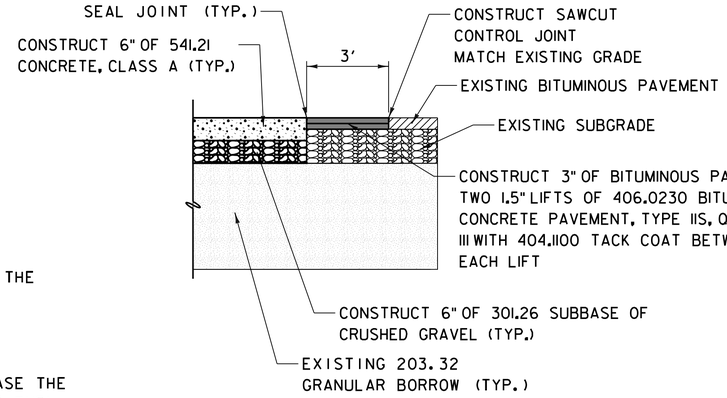
1. ONE CONDUIT SHALL CONTAIN THE FUEL FARM POWER PANEL FEEDER CABLES AND THE EMERGENCY STOP SHUNT TRIP CABLES. THE OTHER CONDUIT SHALL BE SPARE.

2. A NYLON PULL WIRE SHALL BE INSTALLED IN ALL EMPTY CONDUITS.

3. MINIMUM SIZE AND NUMBER OF CONDUITS SHOWN. THE CONTRACTOR SHALL INCREASE THE NUMBER AND SIZE OF CONDUITS IN THE DUCTBANK IF REQUIRED TO ACCOMODATE THE FUEL FARM FINAL DESIGN. DUCTBANK SHALL CONTAIN ONE 2" SPARE CONDUIT MINIMUM.

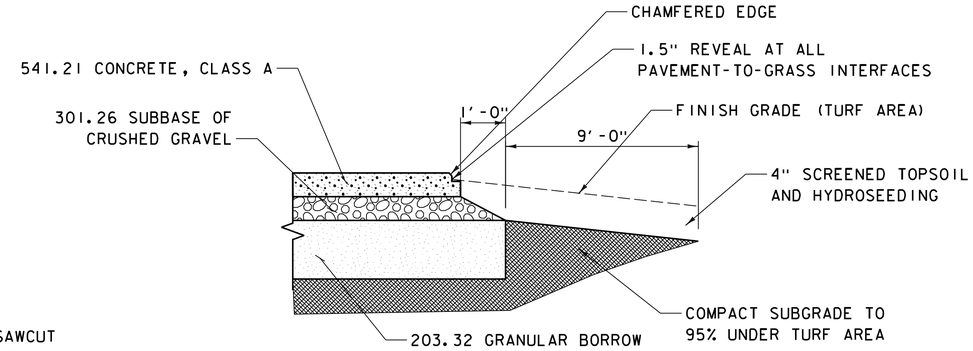
TWO-WAY CONCRETE ENCASED DUCT BANK

NOT TO SCALE



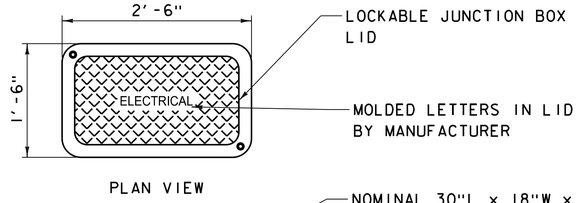
PCC TO BITUMINOUS PAVEMENT TRANSITION

NOT TO SCALE

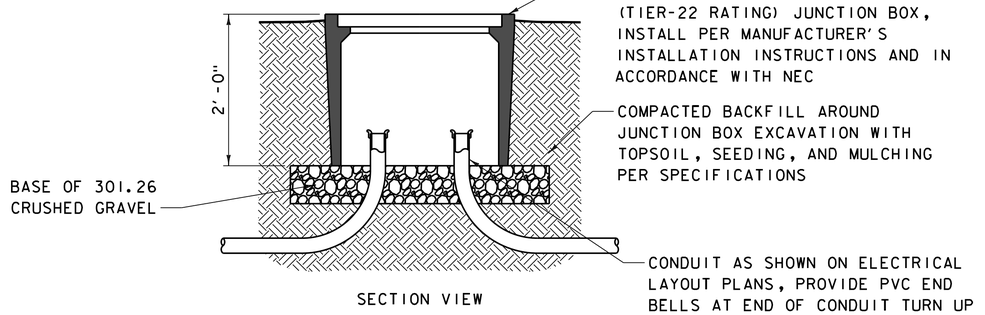


EDGE OF PAVEMENT DETAIL

NOT TO SCALE

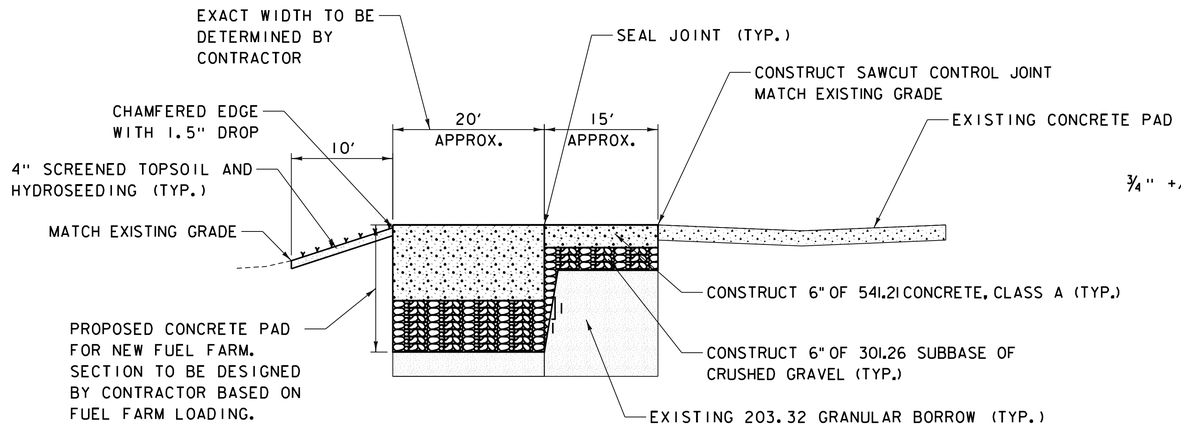


PLAN VIEW



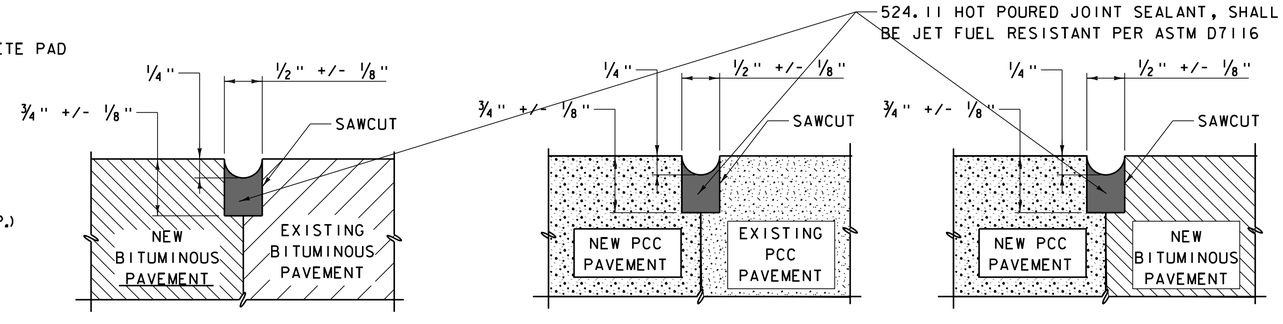
SECTION VIEW

POLYMER CONCRETE HANDHOLE DETAIL



TYPICAL SECTION A-A

NOT TO SCALE



SAW & SEAL JOINT DETAILS

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NOT FOR CONSTRUCTION

Jacobs

PROJECT NAME: MIDDLEBURY	
PROJECT NUMBER: AV-FY23-006	
FILE NAME: z17H107d1101.dgn	PLOT DATE: 8/30/2024
PROJECT LEADER: J. HEHIR	DRAWN BY: S. BAUMANN
DESIGNED BY: S. BAUMANN	CHECKED BY: J. HEHIR
MISCELLANEOUS DETAILS - 1	SHEET 4 OF 5

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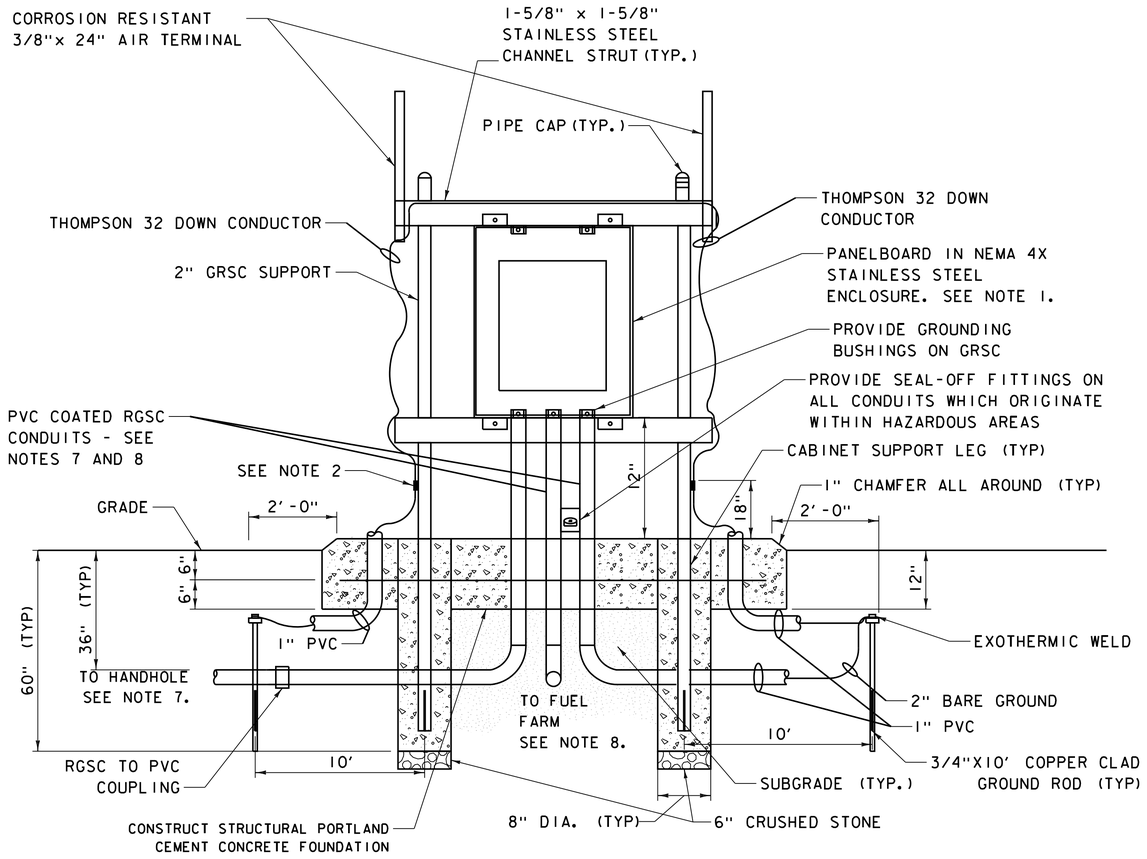
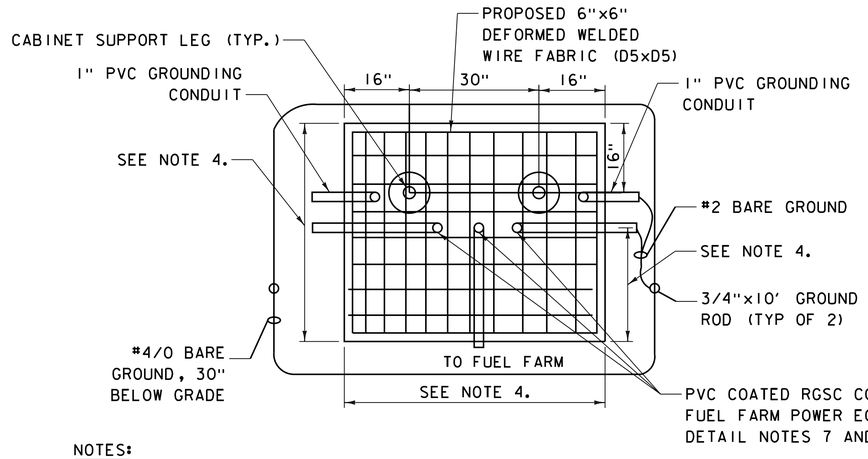


NOTES:

1. THE POWER EQUIPMENT RACK DETAIL SHOWN REPRESENTS MINIMUM DESIGN REQUIREMENTS. THE CONTRACTOR SHALL ADJUST THE SIZE AND RATINGS OF EQUIPMENT, SIZE OF SUPPORT RACK AND FOUNDATION, NUMBER AND SIZE OF CONDUITS,CONDUCTORS, GROUNDING AND LIGHTNING PROTECTION EQUIPMENT AS REQUIRED TO ACCOMMODATE THE FINAL DESIGN.
2. PROVIDE NEW 100A MINIMUM, 120/240V, 3W, SINGLE PHASE 30 POLE MINMUM. POWER PANELBOARD. MOUNT WITHIN NEMA 4X STAINLESS STEEL ENCLOSURE WITH BACK PANEL, HINGED DOOR, DOOR STOP AND PADLOCK HASP. PANEL BUS AND MAIN CIRCUIT BREAKER RATING TO BE DETERMINED BY THE CONTRACTOR AS REQUIRED TO ACCOMMODATE FUEL FARM LOADS WITH ADDITIONAL 25% SPARE CAPACITY FOR FUTURE LOADS. PROVIDE SHUNT TRIP MAIN CIRCUIT BREAKER FOR INTERCONNECTION WITH FUEL FARM EMERGENCY STOP SWITCH. PROVIDE CIRCUIT BREAKERS AS REQUIRED FOR ALL BRANCH CIRCUITS SUPPORTING FUEL FARM LOADS AND PROVIDE 25% SPARE IP-20A CIRCUIT BREAKERS. PANELBOARD SHALL INCLUDE INTEGRAL TVSS UNIT. PANELBOARD SHALL BE AS MANUFACTURED BY EATON, SQUARE D, ABB, OR AN APPROVED EQUAL.
3. TRANSITION LIGHTNING PROTECTION DOWN CONDUCTOR TO #2AWG BARE COPPER AT 18" ABOVE FINISHED GRADE VIA EXOTHERMIC WELD (TYP. 2 PLACES).
4. EXACT LOCATION OF FUEL FARM POWER EQUIPMENT RACK SHALL BE DETERMINED BY THE CONTRACTOR AS REQUIRED TO ACCOMMODATE FINAL DESIGN. POWER EQUIPMENT RACK SHALL BE LOCATED OUTSIDE OF ALL HAZARDOUS AREAS AS DEFINED IN NFPA 70 ARTICLE 500.
5. INSTALLATION SHALL MEET ALL APPLICABLE REQUIREMENTS OF NFPA 70.
6. ALL WIRING BETWEEN PANELBOARD AND FUEL FARM LOADS (OTHER THAN CONTROL EQUIPMENT THAT MAY BE ALSO LOCATED ON THE EQUIPMENT RACK) SHALL BE ROUTED VIA UNDERGROUND CONDUIT. UNDERGROUND CONDUITS AND WIRING BETWEEN THE PANELBOARD AND FUEL FARM LOADS SHALL NOT UTILIZE THE HANDHOLE IN ORDER TO ENSURE PROPER BOUNDARY BETWEEN HAZARDOUS AND NON-HAZARDOUS AREAS IS MAINTAINED.
7. THE NUMBER AND SIZE OF UNDERGOUND CONDUITS BETWEEN THE HANDHOLE AND POWER PANELBOARD SHALL BE DETERMINED BY THE CONTRACTOR AS NECESSARY TO SUPPORT THE FUEL FARM FINAL DESIGN BUT SHALL AT A MINIMUM INCLUDE (1) 2" CONDUIT FOR INCOMING POWER FEEDER TO PANELBOARD AND EMERGENCY STOP SHUNT TRIP WIRING, AND (2) 2" CONDUITS SPARE.
8. THE NUMBER AND SIZE OF UNDERGROUND CONDUITS BETWEEN THE FUEL FARM PANELBOARD AND FUEL FARM LOADS SHALL BE DETERMINED BY THE CONTRACTOR AS NECESSARY TO SUPPORT THE FUEL FARM LOADS.
9. LIGHTNING PROTECTION INSTALLATION SHALL MEET THE APPLICABLE REQUIREMENTS OF NFPA 780.
10. ALL HARDWARE FOR MOUNTING EQUIPMENT AND ERECTING POWER EQUIPMENT RACK SHALL BE STAINLESS STEEL.

FUEL FARM POWER EQUIPMENT RACK
NOT TO SCALE



NOTES:

1. BOND THE ENCLOSURE TO THE LIGHTNING DOWN CONDUCTOR.
2. PROVIDE GROUNDING BRAID BETWEEN THE ENCLOSURE BODY AND DOOR. BOND TOGETHER WITH BACK PANEL, AND CONDUIT BUSHINGS.
3. TOP OF ALL STEEL COUPLINGS SHALL PROTRUDE 1/4" ABOVE THE TOP OF THE CONCRETE FOUNDATION.
4. PAD DIMENSIONS, WHERE SHOWN, INDICATE MINIMUMS. CONTRACTOR SHALL INCREASE THE SIZE OF THE PAD IF ADDITIONAL SPACE IS REQUIRED FOR EQUIPMENT. PAD SHALL EXTEND MINIMUM 36" FROM THE FACE OF THE POWER PANELBOARD TO PROVIDE WORKING CLEARANCE PER NFPA 70.
5. ORIENT PAD SUCH THAT THE FUEL FARM PANELBOARD IS FACING EAST (TOWARD FUEL FARM CONCRETE PAD).
6. ALL UNDERGROUND GROUNDING CONNECTIONS SHALL BE MADE VIA EXOTHERMIC WELD.

CONCRETE FOUNDATION PAD
NOT TO SCALE

NOT FOR CONSTRUCTION

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MISCELLANEOUS DETAILS - 2	SHEET 5 OF 5

Jacobs