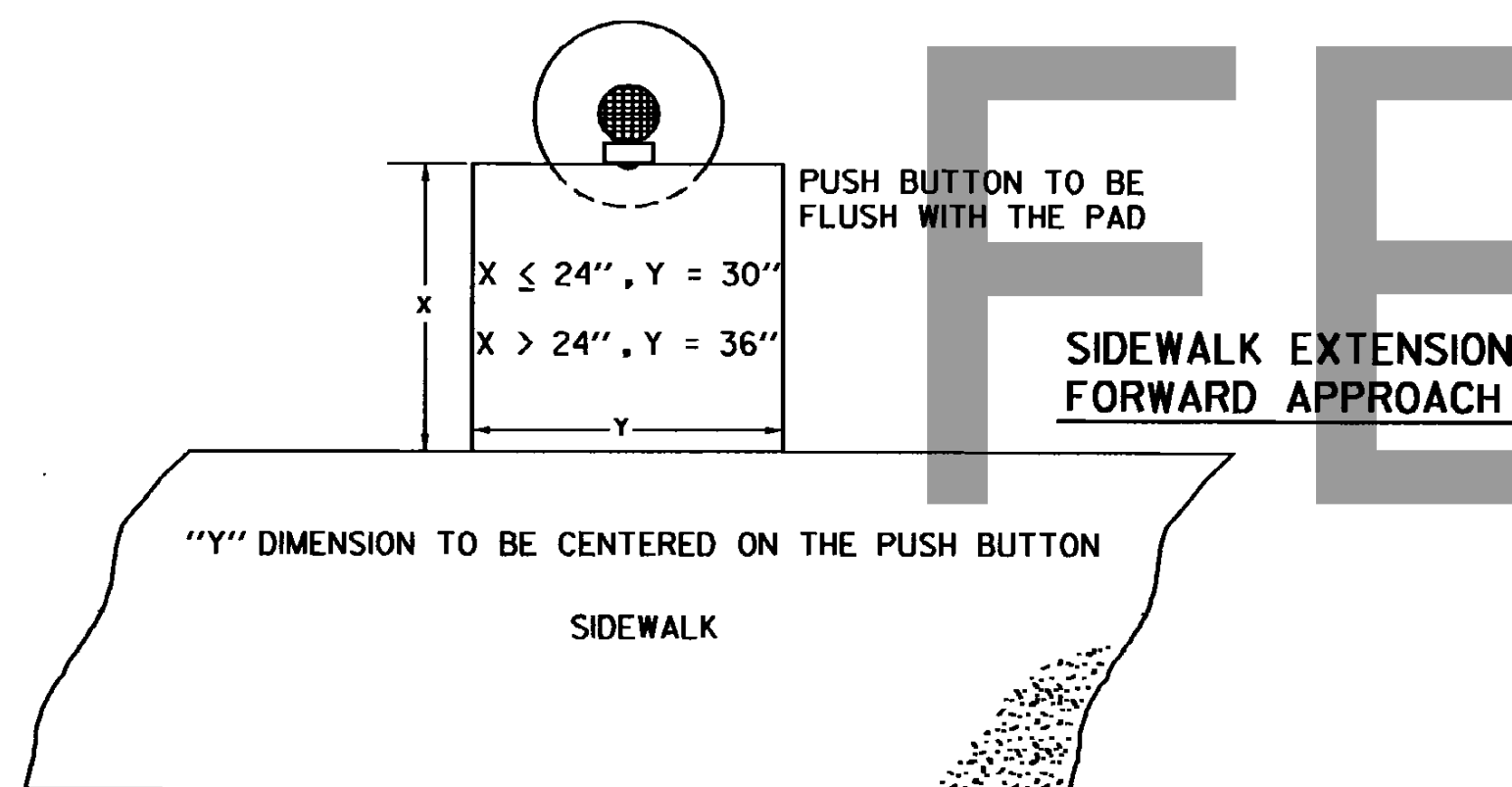
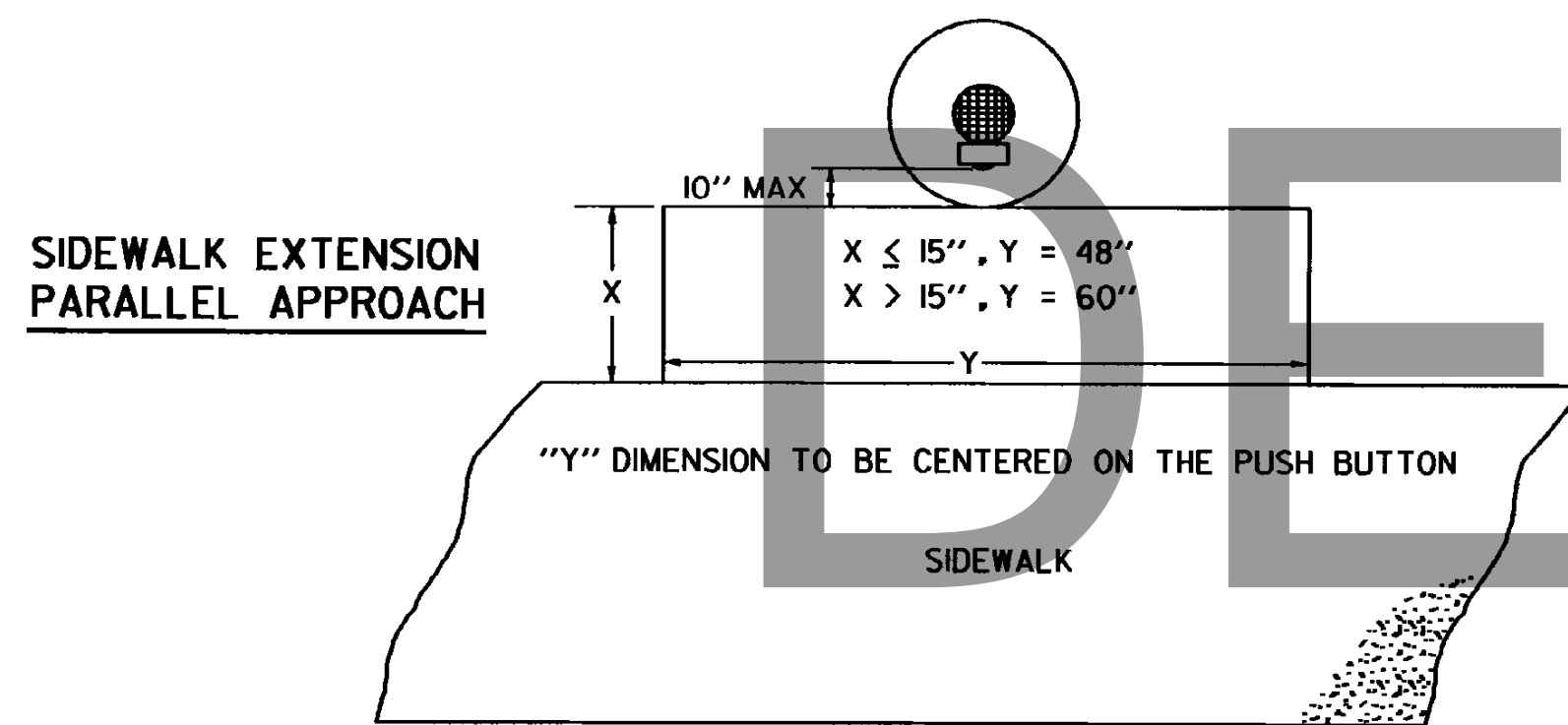
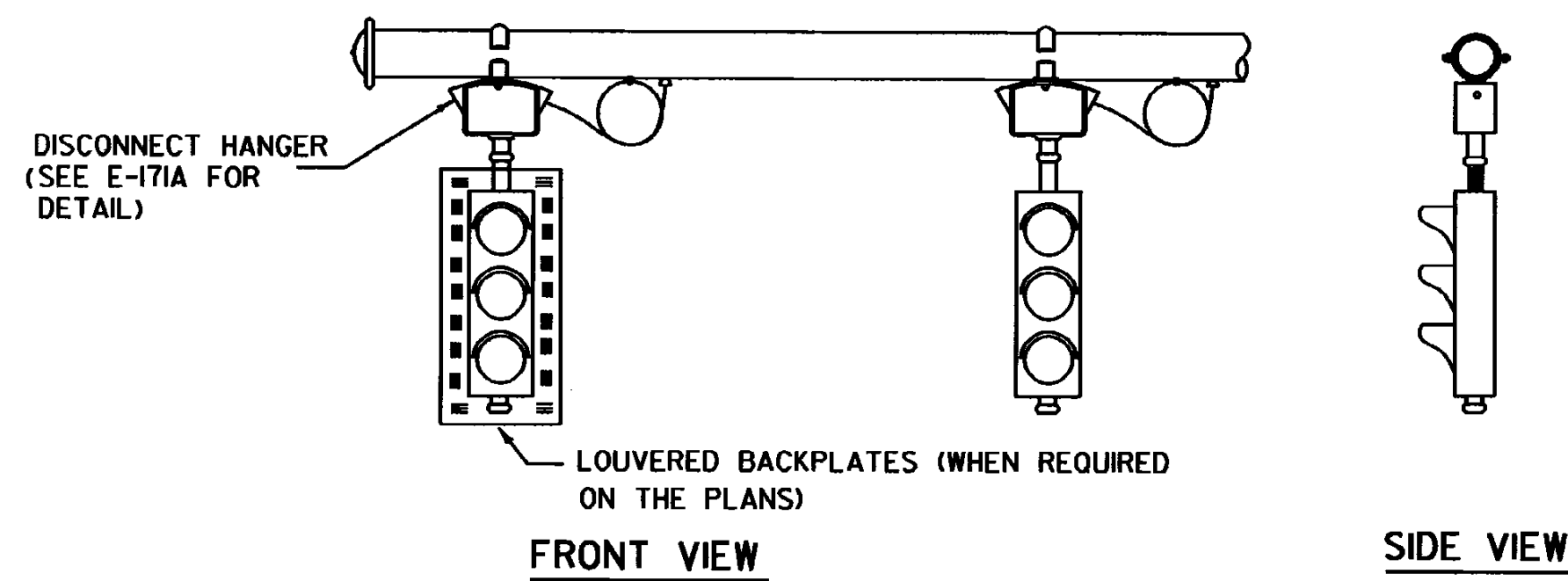


IF THE PEDESTRIAN PUSH BUTTON IS NOT LOCATED WITHIN 10' OF THE SIDEWALK, AN EXTENSION TO THE SIDEWALK MUST BE PROVIDED AS SHOWN BELOW. BUTTONS LOCATED ON THE SIDE OF CONTROLLER CABINETS MAY REQUIRE EXTENSION OF THE CONCRETE PAD, SHOWN ON STD E-171B, TO MEET THIS REQUIREMENT.

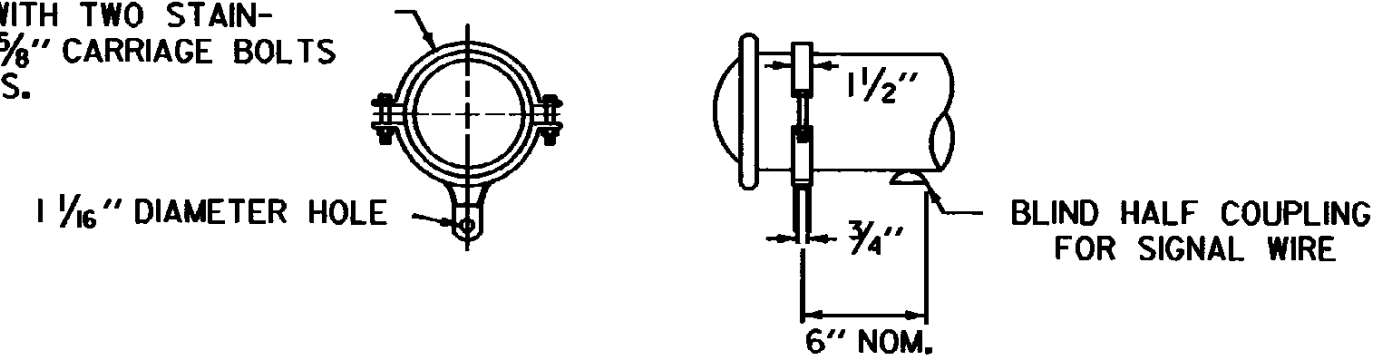
WHEN SIDEWALK EXTENSIONS ARE PROVIDED, A SUBBASE OF FINE GRADED CRUSHED GRAVEL SHALL BE PLACED AND ASPHALT IMPREGNATED FELT SHALL BE USED BETWEEN THE EXTENSION AND THE SIDEWALK AS SHOWN ON STD E-171B.



PEDESTRIAN PUSH BUTTON ACCESSIBILITY DETAIL



CLAMP AND CLEVIS 1/4" STEEL GALVANIZED, WITH TWO STAINLESS STEEL 3/8" CARRIAGE BOLTS AND HEX NUTS.

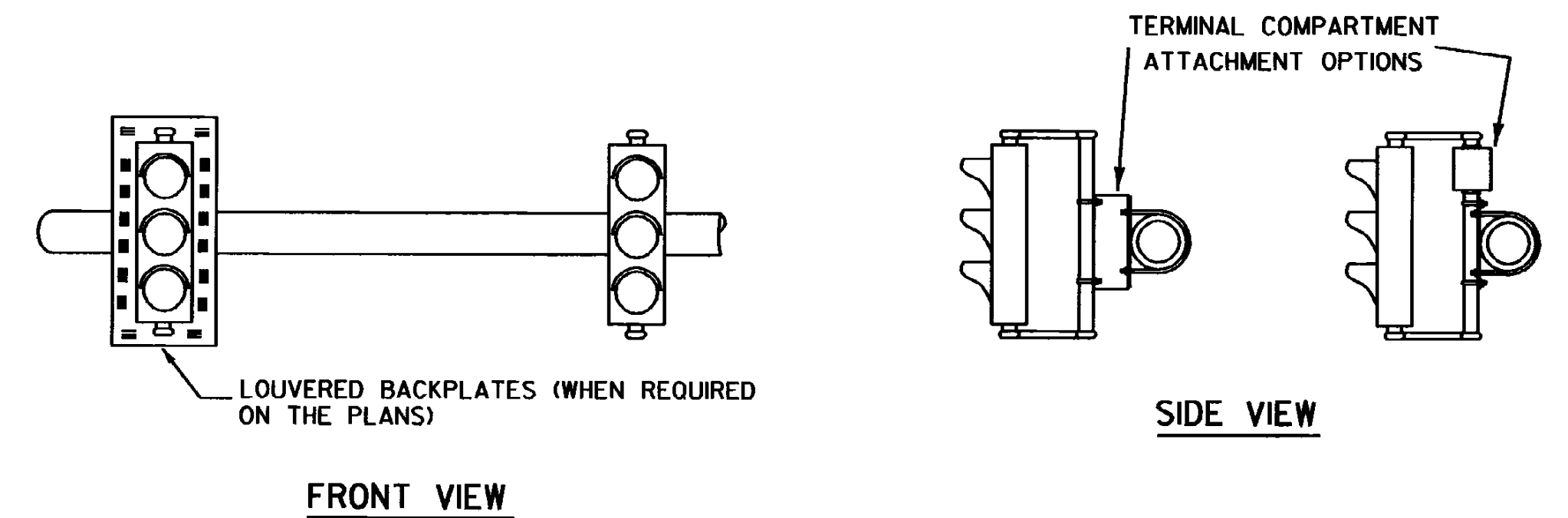


MAST ARM MOUNTING DETAILS FOR FREE SWINGING TRAFFIC SIGNALS

CANTILEVER MOUNTED TRAFFIC SIGNALS (AND LUMINAIRES)

MINIMUM SIGNAL CLEARANCE SHALL BE:
16.5' FOR FREE SWINGING HEADS, OR
17' FOR FIXED MOUNTED HEADS

SEE THE CANTILEVER / FOOTING DETAIL SHEET(S) FOR ADDITIONAL INFORMATION.



SIGNAL MOUNTING PLATE SCHEDULE			
ARM DIAMETER	A	B	C
3 1/16" - 5 3/16"	8 3/4"	5 3/8"	1 1/16"
5 3/16" - 7 7/16"	10 3/4"	7 3/8"	1 3/16"
7 7/16" - 11 3/16"	14"	10 7/8"	1 3/8"

1/2" U-BOLTS COMPLETE WITH HEX NUTS AND FLAT WASHERS

1/2" U-BOLTS COMPLETE WITH HEX NUTS AND WASHERS (SEE SCHEDULE COLUMN "B" FOR RANGES)

ARM DIAMETER RANGE (PER SCHEDULE)

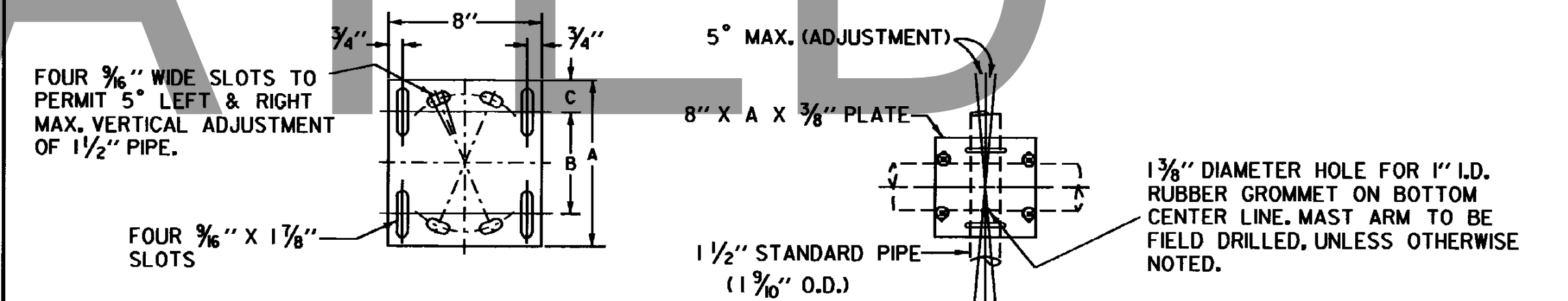


PLATE DETAIL

1. ALTERNATE METHODS FOR RIGID MOUNTING OF SIGNALS TO MAST ARMS MAY BE USED. SHOP DRAWINGS FOR THE ALTERNATE METHOD HARDWARE SHALL BE SUBMITTED TO THE TRAFFIC DESIGN SECTION VIA THE RESIDENT ENGINEER FOR APPROVAL PRIOR TO CONSTRUCTION.
2. ALL RIGIDLY MOUNTED TRAFFIC AND PEDESTRIAN SIGNALS SHALL BE PROVIDED WITH REINFORCEMENT PLATES AT THE ATTACHMENT POINTS.
3. ALL NUTS, BOLTS AND WASHERS SHALL BE STAINLESS STEEL.
4. ALL OTHER MOUNTING BRACKET MATERIALS SHALL BE GALVANIZED STEEL.

MAST ARM MOUNTING DETAILS FOR FIXED MOUNT TRAFFIC SIGNALS (PREFERRED METHOD)

OTHER STDS. E - 170 , E - 171A , E - 171B
REQUIRED

REVISIONS AND CORRECTIONS

NOV. 17, 1993 - DATE OF ORIGINAL ISSUE

AUG. 9, 1995 - ADDED PED. PUSH BUTTON ACCESSIBILITY DETAIL, REMOVED GENERAL CANTILEVER DETAIL, MINOR CORRECTIONS

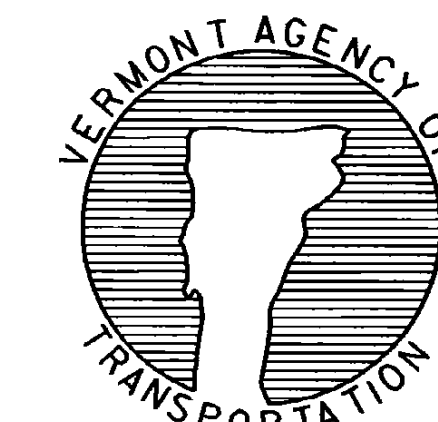
APPROVED FOR THIS PROJECT AND/OR DESIGN IMPLEMENTATION. FHWA FINAL APPROVAL PENDING.

APPROVED

Stephen D. MacArthur
DIRECTOR OF ENGINEERING

David A. Ross
TRAFFIC AND SAFETY ENGINEER

**TRAFFIC CONTROL SIGNALS
CANTILEVER MOUNTING DETAILS
PED. PUSH BUTTON ACCESSIBILITY DETAIL**



**STANDARD
E-171C**